

## Article 10. Off-Street Parking & Loading

- 10.1 GENERAL REQUIREMENTS
- 10.2 LOCATION OF REQUIRED OFF-STREET PARKING
- 10.3 OFF-STREET PARKING FACILITY DESIGN STANDARDS
- 10.4 RESIDENTIAL OFF-STREET PARKING SPACES
- 10.5 REQUIRED OFF-STREET VEHICLE AND BICYCLE PARKING SPACES
- 10.6 PARKING FLEXIBILITIES, EXEMPTIONS, AND REDUCTIONS
- 10.7 BICYCLE PARKING STANDARDS
- 10.8 REQUIRED OFF-STREET LOADING SPACES
- 10.9 COMMERCIAL AND RECREATIONAL VEHICLE STORAGE

### 10.1 GENERAL REQUIREMENTS

#### A. Existing Facilities

1. The existing number of off-street parking and loading spaces may not be reduced below the requirements of this Ordinance. If the number of such existing spaces is already less than required, it may not be further reduced.
2. Existing off-street parking and loading areas that do not conform to the requirements of this Ordinance, but were in conformance with the requirements at the time the parking or loading facilities were established, are permitted to continue as a nonconforming site element.
3. If a building permit for a structure was issued prior to the effective date of this Ordinance, the number of off-street parking and loading spaces must be provided in the amount required at the issuance of the building permit unless the amount required by this Ordinance is less, in which case only the number required by this Ordinance must be installed subject to site plan modification.

#### B. Car- and Bike-Share Facilities

1. Parking facilities may include designated parking spaces for car-share programs. Spaces reserved for car-share programs may count toward minimum parking requirements of this Ordinance.
2. During site plan review by the Planning Board, if a parking facility includes bike-share spaces, the Planning Board may credit such bike spaces toward the minimum parking requirements.

#### C. Completion of Off-Street Parking and Loading Facilities

All off-street parking and loading facilities must be completed prior to the issuance of the certificate of occupancy for the use.

#### D. Use of Off-Street Parking and Loading Facilities

1. The sale, repair, dismantling or servicing of any vehicles, equipment, materials, or supplies is prohibited. The sales and display of goods in off-street parking areas is also prohibited unless specifically permitted by this Ordinance.
2. The property owner is responsible for ensuring that parking and loading facilities are used only by tenants, employees, visitors, and other authorized persons.
3. Space allocated to any off-street loading space may not be used to satisfy the requirement for any off-street vehicle parking space or access aisle, or portion thereof. Conversely, the area allocated to any off-street vehicle parking space may not be used to satisfy the requirement for any off-street loading space or portion thereof.

## 10.2 LOCATION OF REQUIRED OFF-STREET PARKING

### A. Residential Uses

1. All required parking spaces for residential uses must be located on the same lot as the residential use. Tandem parking is permitted, but both spaces must be allotted to the same dwelling unit and located on the same lot as the dwelling.
2. Required parking spaces for residential uses may be located on a driveway that provides access to a garage or other off-street parking space. Vehicles parked within a driveway or off-street parking space cannot project over the sidewalk or right-of-way.

### B. Nonresidential Uses

1. Required off-street parking spaces for nonresidential uses in residential districts must be located on the same lot as the use served, except as allowed in this section.
2. Off-street parking spaces for nonresidential uses in the mixed-use and commercial districts may be located on a lot not more than 600 feet from the lot served provided: When located on a lot not owned by the business, control must be established by a written agreement specifying that such parking will remain available for the entire time period the use is in operation.
3. Valet service to a parking facility with no distance restriction. If the valet service is discontinued, the use must provide the required parking within 30 days. Site plan review by the Planning Board is required to approve the new parking arrangement.
4. Parking spaces for the NCU, AC, UN, NC, and UC Districts cannot be located in front of the front building line.
5. In the GC-U and GC-R Districts, no more than 20% of parking spaces provided may be located in front of the front building line.

## 10.3 OFF-STREET PARKING FACILITY DESIGN STANDARDS

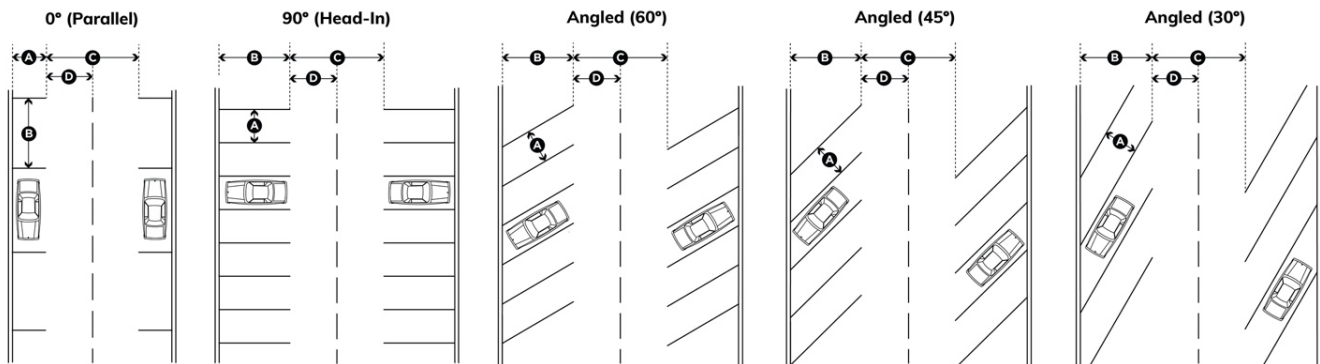
The following standards apply to off-street parking facilities, both parking lots and parking structures, as applicable, with the exception of single-family, two-family, and townhouse dwellings, which are subject to the standards of Section 10.4. However, if a townhouse dwelling or cottage court development is designed with a common parking lot or parking structure, including multiple common parking lots or structures for all tenants/residents, then it is subject to all the standards of this section.

### A. Dimensions

1. Off-street parking spaces and drive aisles within a parking lot or structure must be designed in accordance with the minimum dimensions in Table 10-A: Off-Street Parking Space Minimum Dimensions. Other parking angles other than those described in Table 10-A are permitted but must be approved by the Zoning Officer and provide evidence of safe and efficient parking configuration and traffic circulation.
2. If additional spaces are provided for motorcycle and scooter parking spaces, such spaces must measure at least four feet in width by eight feet in length and must be identified or designated through the use of signs or pavement markings.

Table 10-A: Off-Street Parking Space Minimum Dimensions					
Parking Angle	Stall Width (A)	Stall Depth (B)	Aisle Width Two-Way (C)	Aisle Width One-Way (D)	Vertical Clearance
0° (Parallel)	9'	20'	24'	12'	7'
90° (Head-In)	9'	18'	24'	24'	7'
60°	9'	21'	24'	19'	7'
45°	9'	17'	24'	13'	7'
30°	9'	17'	22'	11'	7'

**A** Stall Width    **B** Stall Depth    **C** Aisle Width (Two-Way)    **D** Aisle Width (One-Way)



**C. Accessible Spaces**

Accessible parking spaces must be at least eight feet wide with an adjacent aisle at least eight feet wide. Two accessible parking spaces may share a common access aisle. Accessible parking must be provided in accordance with Table 10-B: Required Accessible Parking. Each accessible parking space must be marked with ADA-compliant signage and any other applicable standards of the Americans with Disabilities Act (ADA).

Table 10-B: Required Accessible Parking	
Total Parking Spaces in Lot or Garage	Number of Accessible Parking Spaces
1 to 25	1
26 to 50	2
51 to 75	3
76 to 100	4
101 to 150	5
151 to 200	6
201 to 300	7
301 to 400	8
401 to 500	9
501 to 1,000	2% of total
Over 1,000	20 plus 1 for each 100 over 1000

**D. Access**

1. All required off-street parking facilities must have vehicular access from a street, driveway, alley, or cross-access connection.
2. All required off-street parking facilities must have an internal pedestrian circulation system that allows for safe passage between parking areas and any public sidewalk in the adjacent right-of-way and the use it serves. This includes, but is not limited to, interconnected sidewalks, striped walkways, and separated walkways.

3. All parking facilities must be designed with vehicle egress and ingress points that least interfere with traffic movement. Parking facilities must be designed to allow the driver to proceed forward into traffic, rather than back out.
4. All driveways to parking facilities must comply with the following:
  - a. For off-street parking facilities of more than four spaces, access drives must have a minimum width of ten feet for one-way traffic
  - b. For off-street parking facilities of more than four spaces, access drives must have a minimum width of 22 feet for two-way traffic. However, a ten foot wide driveway is permitted for two-way traffic when all of the following are met:
    - i. The driveway does not exceed 50 feet.
    - ii. It provides access to no more than ten spaces.
    - iii. Sufficient turning space is provided so that vehicles do not back into a public street.
  - c. Access drives to off-street parking facilities cannot occupy more than 25% of the total lot frontage with the following exception: on lots with more than 150 feet of frontage, the aggregate width of such drives cannot exceed 48 feet for each 300 feet of lot frontage.
5. Dead end parking lots without a turnaround space are prohibited. A turnaround space must have a minimum depth and width of nine feet, and must be designated with signs stating “No Parking” and painted to indicate parking is prohibited.

#### **E. Surfacing**

All off-street parking lots must be improved with a hard surfaced, all-weather dustless material; pervious paving is encouraged and may also be used. Gravel is prohibited.

#### **F. Striping**

All off-street parking facilities must delineate parking spaces with paint or other permanent materials, which must be maintained in clearly visible condition.

#### **G. Curbing**

1. Curbing is required when a parking space abuts a pedestrian walkway, landscape area, structure, or fence. Inlets, or breaks in curbing may be provided to allow for drainage into landscape areas that can absorb water. Curbing must be constructed of permanent materials such as concrete, stone, or masonry, a minimum height of four inches above ground level, and permanently affixed to the paved parking area.
2. Wheel stops are permitted only as part of a curb-less parking lot design that allows for sheet flow to move directly into a bio-retention area. In such a design, a stone buffer area is required between the paved surface and planting area to slow sheet flow and minimize erosion into the absorbent areas.

#### **H. Lighting**

Parking lot and structure lighting must be in accordance with Section 9.4.

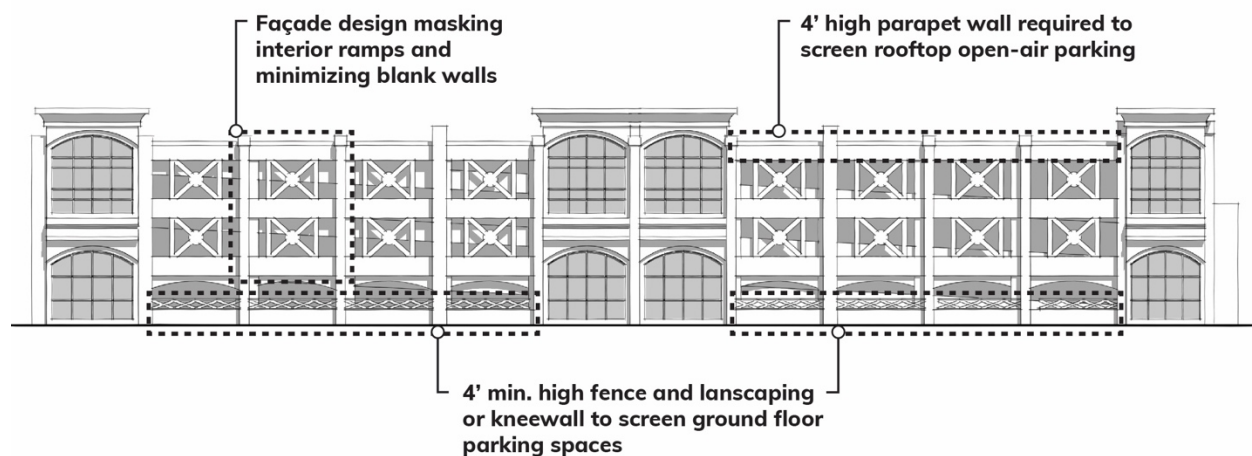
#### **I. Landscape and Drainage**

1. All parking lots and structures must be landscaped in accordance with Article 11.
2. All parking areas must be adequately drained. All lots with more than four parking spaces must have drainage connected to a public storm sewer if located within 500 feet of an available public storm sewer system.

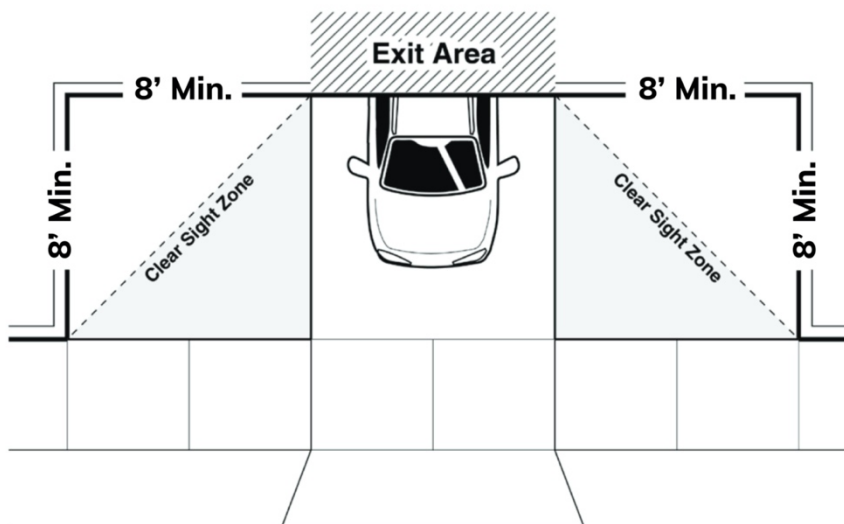
## J. Parking Structure

1. Parking structures located in the GC-U, UN, NC, and UC Districts that have frontage along a public street are required to meet one of the following design standards:
  - a. Liner buildings for nonresidential uses along a minimum of 60% of the length of a façade adjacent to a public street. Nonresidential spaces must be a minimum of 20 feet in width and 20 feet in depth, with utility stubs.
  - b. Public green space or civic space, designed in accordance with the standards in Section 9.2, located along the full length of the façade, with the exception of one access point, and a minimum of 30 feet in depth.
2. On facades that front on public streets, façade design and screening must mask interior ramps.
3. Parking structures must be designed to minimize blank facades through architectural detail and landscape.
3. On portions of the ground floor façade where parking spaces are visible, a decorative fence and landscape or a kneewall is required to screen parking spaces. Such fence or kneewall must be a minimum of four feet in height.
4. For parking structures with rooftop open-air parking, a four foot parapet wall is required for screening.
5. A vehicular clear sight zone must be included at vehicular exit areas as follows:
  - a. The ground floor façade of a parking structure must be setback a minimum of eight feet from a public sidewalk or pedestrian walkway at the vehicular access point.
  - b. An additional minimum eight foot ground floor façade setback is required at the edge of the access point for a minimum of eight feet. This is determined by drawing a line from the edge of the vehicular access point along the abutting the public sidewalk or pedestrian walkway. In this area, groundcover, landscape, or decorative wall must be used to act as a buffer between the access aisle and the public sidewalk or pedestrian walkway. Landscape or a decorative wall must not exceed 30 inches in height to maintain driver sightlines to pedestrians.
  - c. The upper story façade(s) of the parking structure may overhang the vehicular clear sight zone.

### PARKING STRUCTURE - DESIGN



#### PARKING STRUCTURE - CLEAR ZONE



#### 10.4 RESIDENTIAL OFF-STREET PARKING SPACES

The following apply to single-family, two-family, and townhouse dwellings. However, if a townhouse dwelling or cottage court development is designed with a common parking lot or parking structure, including multiple common parking lots or structures for all residents/tenants, then it is subject to the standards of Section 10.3.

- A. All required off-street parking spaces must have vehicular access from a driveway that connects to a street or alley.
- B. Townhouse developments are prohibited from constructing individual curb cuts for each dwelling unit along a public street.
- C. Required parking spaces to single-family and two-family dwellings may be designed so that the driver may back out into traffic.
- D. A residential driveway is limited to 25% of the lot width or 25 feet, whichever is less.
- E. A residential driveway may be located one foot from any interior side or corner side lot line, unless a shared driveway is established. A shared driveway location is only allowed if agreed to by the owners of each lot, and the agreement is recorded as a shared driveway easement on each plat of survey.
- F. All off-street parking spaces and parking pads must be improved with a hard surfaced, all-weather dustless material. Pervious paving is encouraged and may also be used.
- G. For single-family and two-family dwellings, a parking space may consist of two parallel paved parking strips, each of which is at least 18 inches in width and 18 feet long.

#### 10.5 REQUIRED OFF-STREET VEHICLE AND BICYCLE PARKING SPACES

##### A. Required Spaces

Table 10-C: Off-Street Parking Requirements states the minimum number of off-street parking spaces to be provided for the designated uses. Table 10-C lists parking requirements for the uses listed within the districts. In some cases, uses that are considered part of a generic use category are listed with specified parking requirements. These specific uses are listed only for the purposes of this section and do not indicate whether such uses are permitted or special uses within any district. Certain uses listed within the districts may not have parking requirements and others may have specific requirements listed within the use standards of Article 8.

### **B. Calculation of Required Spaces**

The total number of required parking spaces is calculated by the principal use of the lot. When more than one use occupies the same lot, the number of required spaces is the sum of the separate requirements for each use, unless a shared parking arrangement is approved or the standards of items C and D below apply. The following standards for computation apply:

1. In determining the number of parking spaces, when the result contains a fraction, any fraction less than one-half is disregarded and any fraction of one-half or more is counted as one parking space.
2. In places of assembly in which patrons or spectators occupy benches, pews, or similar seating facilities, each 24 inches of such seating facility is counted as one seat for the purpose of determining the requirement for off-street parking facilities. Floor area of a prayer hall is counted as one seat per marked prayer mat space or one seat for every five square feet in the prayer hall if prayer mat spaces are not marked.

### **C. Calculation for Age-Restricted Housing**

A multi-family dwelling or a planned residential community of various dwelling types designated as age-restricted housing is subject to the following standards:

1. One parking space per dwelling unit.
2. One bicycle parking space per eight dwelling units, with a minimum of 80% of such bicycle spaces designed as long-term bicycle parking spaces.

### **D. Calculation for Multi-Tenant Commercial Centers**

Parking for multi-tenant commercial centers is calculated as one space required per 500 square feet of gross floor area, rather than by the individual uses. A multi-tenant commercial center is defined as a group of three or more commercial establishments, primarily retail, but also including service, restaurant, recreation, office, or medical, that is planned, owned, and/or managed as a single property. The two main configurations of multi-tenant commercial centers are large shopping centers and strip retail centers. A mixed-use development that includes one or more floors of commercial establishments also qualifies for this calculation; the residential component is calculated separately.

### **E. Bicycle Parking**

Certain uses listed within Table 10-C are required to provide bicycle parking spaces. Bicycle parking spaces are required only for new construction as of the effective date of this Ordinance.

1. Of those uses required to provide bicycle spaces, some are required to provide long-term spaces, where bicycles will be left for longer periods of time and require a safe and weatherproof storage area. The required number of long-term spaces is shown as a percentage of the required total bicycle spaces. All other required bicycle spaces must be designed as short-term spaces, which are areas where bicycles will be left for short stops, requiring a high degree of convenience.
2. In all cases where bicycle parking is required, a minimum of two bicycle spaces must be provided. After the first 25 required bicycle parking spaces are provided, any additional bicycle parking spaces are required at a 50% reduction.
3. Where bicycle parking space requirements indicate "Over 10,000sf GFA" or other number threshold, this means that bicycle spaces are required only for structures over a certain gross floor area. In such case, bicycle parking space requirements are calculated on the basis of the entire gross floor area.
4. When a use is exempt from providing vehicle parking it is still required to provide the required bicycle spaces.

### **F. Required Electric Vehicle Charging Stations**

1. For nonresidential and mixed-use parking facilities of 30 or more spaces, a minimum of 2% of the spaces, with any fraction rounded up, must be constructed to accommodate electrical vehicle charging stations. A minimum of one space must be ADA accessible. During site plan review, the required number of ADA accessible electric vehicle charging spaces may be increased.
2. There must be a raceway system from the electrical panel to the spaces for electric vehicle charging. The associated electrical equipment room must have dedicated space to install the required equipment for electric vehicle charging.

3. All for commercial parking facilities, all spaces and associated electrical infrastructure must be constructed for either level 2 charging stations or level 3 fast charging stations, or the equivalent if standard industry categorizations change.
4. The standards of Section 9.4.I must also be met; however, in case of conflict with this section, this section controls.

**G. Vehicle Parking Maximums**

The number of vehicle parking spaces cannot exceed 120% of the required minimum unless waived by the Planning Board.

TABLE 10-C: OFF-STREET VEHICLE AND BICYCLE PARKING REQUIREMENTS			
USE	MINIMUM REQUIRED VEHICLE SPACES	MINIMUM REQUIRED BICYCLE SPACES	
		REQUIRED TOTAL BICYCLE SPACES	% OF REQUIRED BICYCLE SPACES FOR LONG-TERM SPACES
Adult Care Facility	0.5 per dwelling unit or room		
Adult Use	1 per 300sf GFA		
Amusement Facility, Indoor	1 per 500sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Amusement Facility, Outdoor	1 per 1,000sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Animal Care Facility – Large Animal	1 per 500sf GFA		
Animal Care Facility – Small Animal	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Animal Grooming Establishment	1 per 500sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Art Gallery	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Art and Fitness Studio	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Bed and Breakfast	2 spaces + 1 per guestroom		
Body Modification Establishment	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Broadcasting Facility	1 per 1,000sf GFA of studio and office area	Over 10,000sf GFA: 1 per 2,500sf GFA	
Campground	2 per campsite	2 per campsite	
Car Wash	1 per car wash bay + 4 stacking spaces per bay		
Cemetery	1 per 200sf of GFA of office and chapel/parlor		
Children's Home	1 per 200sf of GFA of office area		
Commercial Kitchen (Standalone)	3 per kitchen space	Over 5,000sf GFA: 1 per 1,500sf GFA	
Community Center	1 per 300sf GFA	1 per 2,500sf GFA	
Country Club	Calculated as the cumulative number required per facilities offered (golf course, driving range, eating and drinking establishment, etc.)	Calculated as the cumulative number required per facilities offered (golf course, driving range, restaurant, bar, etc.)	
Cultural Facility	1 per 300sf GFA	1 per 2,500sf GFA	
Day Care Center: Child Day Care	1 per 500sf GFA		
Day Care Center: Small Day Care	1 per 500sf GFA		
Day Care Center: Social Adult Day Care	1 per 500sf GFA		
Dormitory	1 per 2 du	1 per 4 du	50%
Drug Treatment Clinic	1 per 300sf GFA		



TABLE 10-C: OFF-STREET VEHICLE AND BICYCLE PARKING REQUIREMENTS			
USE	MINIMUM REQUIRED VEHICLE SPACES	MINIMUM REQUIRED BICYCLE SPACES	
		REQUIRED TOTAL BICYCLE SPACES	% OF REQUIRED BICYCLE SPACES FOR LONG-TERM SPACES
Dwelling – Caretaker	2 spaces		
Dwelling – Manufactured Home	2 spaces		
Dwelling – Multi-Family	2 per du, except 1.5 per du in UR-4, NC, and UN	1 per 4 du	80%
Dwelling – Single-Family	2 per du, except 1.5 per du in UR-4, NC, and UN		
Dwelling – Single-Family - Attached	2 per du, except 1.5 per du in UR-4, NC, and UN		
Dwelling – Townhouse	2 per du, except 1.5 per du in UR-4, NC, and UN	1 per 4 du	80%
Dwelling – Two-Family	2 per du, except 1.5 per du in UR-4, NC, and UN		
Eating and Drinking Establishment	1 per 100sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Educational Facility – Primary or Secondary			
<i>Educational Facility – Elementary and/or Middle</i>	<i>3 per classroom + 1 per 300sf of office area</i>	<i>1 per 8 classrooms</i>	
<i>Educational Facility – High School</i>	<i>6 per classroom + 1 per 300sf of office area</i>	<i>1 per 4 classrooms</i>	
Educational Facility – University or College	10 per classroom + 4 per office	1 per classroom	20%
Educational Facility – Vocational	10 per classroom + 4 per office	1 per 2,500sf GFA	
Family-Type Home for Adults	1 per 2 rooms	1 per 4 rooms	
Financial Institution	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Financial Institution, Alternative	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Food Bank	1 per 300sf of office area		
Food Pantry	1 per 500sf GFA		
Freight Terminal	1 per 300sf of office area		
Funeral Home	1 per 300sf GFA		
Gas Station	2 per pump island + 1 per 500sf GFA of structure + 4 stacking spaces per accessory car wash bay		
Golf Course/Driving Range	4 per golf hole + 2 per tee of driving range	1 per golf hole + 1 per tee of driving range	
Greenhouse/Nursery - Retail	1 per 500sf GFA of indoor space + 1 per 1,000sf of outdoor space		
Health Spa	1 per 500sf GFA	1 per 2,500sf GFA	
Heavy Equipment Sales, Service, and Storage	1 per 500sf GFA of indoor space + 1 per 1,000sf of outdoor display space		
Horse Race Track	1 per 4 seats	1 per 10 seats	
Horse Stable, Commercial	1 per 2 stalls	1 per 10 stalls	
Hospital	1 per 2 beds	1 per 10 beds	20%
Hotel	1.5 per room		
Industrial Design	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Industrial – Artisan	1 per 1,000sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	

TABLE 10-C: OFF-STREET VEHICLE AND BICYCLE PARKING REQUIREMENTS

USE	MINIMUM REQUIRED VEHICLE SPACES	MINIMUM REQUIRED BICYCLE SPACES	
		REQUIRED TOTAL BICYCLE SPACES	% OF REQUIRED BICYCLE SPACES FOR LONG-TERM SPACES
Industrial – General	1 per 1,000sf GFA up to 40,000sf, then 1 per 2,500sf for additional GFA above 40,000sf (excludes any outdoor storage)	Over 10,000sf GFA: 1 per 2,500sf GFA	
Industrial – Light	1 per 1,000sf GFA up to 40,000sf, then 1 per 2,500sf for additional GFA above 40,000sf (excludes any outdoor storage)	Over 10,000sf GFA: 1 per 2,500sf GFA	
Inn	2 spaces + 1 per guestroom		
Live/Work	2 per du	1 per 2 du	80%
Live Performance Venue	1 per 200sf GFA	1 per 2,500sf GFA	
Lodging House	2 spaces + 1 per guestroom		
Manufactured Home Park	2 per manufactured home site	2 per manufactured home site	
Marijuana Dispensary	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Marina	1 per 2 slips	1 per 6 slips	
Medical/Dental Office	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Micro-Production of Alcohol	1 per 500sf GFA	Over 5,000sf GFA: 1 per 2,500sf GFA	20%
Office	1 per 300sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	20%
Outdoor Storage Yard	1 per 2,500sf of lot area		
Passenger Terminal	1 per 1,000sf GFA		
Personal Service Establishment	1 per 500sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Place of Worship	1 per 10 seats	1 per 25 seats	
Private/Social Club	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Reception Facility	1 per 300sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Recreational Vehicle (RV) Park	1 per RV site	1 per 10 RV sites	
Research and Development	1 per 500sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Residential Care Facility	1.2 per patient room		
Retail Goods Establishment	1 per 500sf GFA	Over 10,000sf GFA: 1 per 2,500sf GFA	
Rooming House (Small, Medium, or Large)	2 spaces + 1 per guestroom		
Self-Service Storage Facility	1 per 25 storage units		
Shelter, Domestic Violence	1 per 500sf GFA		
Shelter, Emergency	1 per 500sf GFA		
Shelter, Homeless	1 per 300sf of office area		
Social Service Center	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Solar Energy System - Tier 3	4 spaces		
Specialty Food Service	1 per 500sf GFA	Over 5,000sf GFA: 1 per 1,500sf GFA	
Vehicle Dealership	1 per 500sf of indoor sales and display area + 4 per service bay		
Vehicle Operation Facility	1 per 2,500sf of lot area		

TABLE 10-C: OFF-STREET VEHICLE AND BICYCLE PARKING REQUIREMENTS			
USE	MINIMUM REQUIRED VEHICLE SPACES	MINIMUM REQUIRED BICYCLE SPACES	
		REQUIRED TOTAL BICYCLE SPACES	% OF REQUIRED BICYCLE SPACES FOR LONG-TERM SPACES
Vehicle Rental	1 per 500sf GFA of indoor area (excluding indoor storage of vehicles)		
Vehicle Repair/Service	4 per service bay		
Warehouse	1 per 500sf of office area + 1 per 30,000sf GFA of warehouse		
Wholesale Establishment	1 per 500sf of office area + 1 per 15,000sf GFA of warehouse		
Wind Energy System	1 per turbine		

## 10.6 PARKING FLEXIBILITIES, EXEMPTIONS, AND REDUCTIONS

### A. Exemptions from and Reductions to Parking Requirements

1. As of the effective date of this Ordinance, existing nonresidential structures in the NCU, AC, NC, UN, and UC Districts that currently do not provide the required amount of parking on the lot to accommodate parking are subject to the following:

- a. If the structure does not provide any on-site parking on-site, it is not required to provide any parking regardless of a change in use or intensity of use.
- b. All existing on-site parking located must be maintained. If there is a change in use or intensity of use that requires additional parking in excess of what is currently maintained on-site, site plan review is required and may require provision of additional parking.
- c. If the structure expands their footprint or gross floor area and such expansion requires additional parking from what is currently required on-site, site plan review is required and may require provision of additional parking.
- d. If the lot area is expanded (e.g., the adjoining lot is purchased or leased), this exemption is null and void.
- e. Once the principal building is demolished, this exemption is null and void.

2. The UC District is exempt from minimum vehicle parking requirements. Where parking is provided, property is still subject to basic requirements of the chapter.

3. For nonresidential uses in the commercial and mixed-use districts, on-street parking spaces located along the front or side lot line may be counted toward required off-street parking spaces for commercial uses. New on-street parking spaces may also be created to count toward required off-street parking but must be located along the corner side or front lot line.

- a. Where on-street parking spaces are unmarked, the number of parking spaces is calculated by dividing the length of the on-street parking area located parallel to the lot line of the property under consideration divided by 22, where a fraction of less than one-half is disregarded, and a fraction of one-half or more is counted as one space.
- b. Where on-street parking spaces are marked, each marked space counts as one required parking space, including any space where at least 80% of the width is located along the lot line of the property under consideration.
- c. Spaces accessible 24 hours a day to the public are counted at 100%. Any time restricted spaces or spaces subject to posted alternate side of the street parking programs are counted at 50%.

**B. Shared Parking Permission**

Off-street parking spaces for separate uses may be provided collectively at the applicant's option at a reduced amount of the total number of spaces provided it meets the calculation of Table 10-D: Shared Parking Calculation. Adjacent sites may use the shared parking option but must provide the City with a copy of the agreement between the property owners, and full site access between sites must be granted 24 hours a day.

1. The required number of spaces for each use is calculated according Table 10-C.
2. The required number of spaces for each use is then applied to the percentages for each timeframe, according to the appropriate land use category, in Table 10-D to determine the number of required spaces. This is done for each timeframe category.
3. The numbers are summed for all uses within each timeframe and the highest sum total in a timeframe is the required number of spaces.

Table 10-D: Shared Parking Calculation						
Land Use Category	Weekday			Weekend		
	Mid-7am	7am-6pm	6pm-Mid	Mid-7am	7am-6pm	6pm-Mid
Residential	100%	100%	100%	100%	100%	75%
Commercial	0%	100%	80%	0%	100%	60%
Restaurant	50%	70%	100%	45%	70%	100%
Hotel/Motel	100%	50%	90%	100%	65%	80%
Indoor/Outdoor Recreation	0%	70%	100%	5%	70%	100%
Office /Industrial	5%	100%	5%	0%	40%	10%
Institutional – Educational Facility	0%	100%	50%	0%	0%	0%
Institutional – Place of Worship	0%	50%	0%	0%	100%	0%

**SAMPLE CALCULATION**

Example: multi-use office development with the following uses within the development; based on current parking requirements, the number of required spaces is:

Use & Square Footage	Parking Requirement	Number of Spaces Needed
Retail: 15,000sf GFA	1 per 500sf GFA	30 spaces
Restaurants: 2,000sf GFA	1 per 100sf GFA	20 spaces
Hotel/Motel: 60 rooms	1.5 per room	90 spaces
Office: 24,000sf GFA	1 per 300sf GFA	80 spaces
<b>TOTAL SPACES REQUIRED</b>		<b>220 spaces</b>

Using the shared parking calculation, these numbers are plugged into the table and using the percentages allotted to each land use for each time of day, are calculated as total spaces required per timeframe.

Land Use Category	Weekday			Weekend		
	Mid-7am	7am-6pm	6pm-Mid	Mid-7am	7am-6pm	6pm-Mid
Residential	-	-	-	-	-	-
Commercial	0	20	16	0	20	12
Restaurant	15	21	30	13.5	21	30
Hotel/Motel	90	45	81	90	58.5	72
Indoor/Outdoor Recreation	-	-	-	-	-	-
Office/Industrial	4	80	4	0	32	8
Institutional – Educational Facility	-	-	-	-	-	-
Institutional – Place of Worship	-	-	-	-	-	-
<b>Totals</b>	<b>109</b>	<b>166</b>	<b>131</b>	<b>103.5</b>	<b>131.5</b>	<b>122</b>

With a straight parking calculation, 220 spaces are required. However, the shared parking provision allows this example multi-use office development to be constructed by-right with 166 spaces (the highest number of spaces within the various timeframes - the 7am to 6pm weekday timeframe). This is because these timeframe calculations take into account the times of day the various uses utilize the most parking.

### C. Land Banked Parking

The Planning Board may permit land banking of up to 25% of the required parking spaces through the site plan review process.

1. Sufficient evidence is provided by the applicant that supports the reduced parking needs.
2. The area proposed for land banking of parking spaces is suitable for parking at a future time.
3. The land-banked area must, at a minimum, be landscaped with live groundcover. As a result of site plan review, additional landscaping of the land-banked area may be required.
4. The land banked area cannot be used for any other use. The land banked parking area cannot be used to fulfill other landscaping requirements of this Ordinance.
5. As part of the site plan review process, the applicant must show the area to be banked on the site plan and marked as "Land-Banked Future Parking."
6. The Zoning Officer, on the basis of increased parking demand for the use, will require the conversion of all or part of the land-banked area to off-street parking spaces. Nothing prevents the applicant from converting the land banked area to parking prior to City notification.

## 10.7 BICYCLE PARKING SPACES

### A. Design

1. Required bicycle spaces must provide each bike space within a row of bicycle parking a minimum of two feet in width by six feet in length, with a minimum vertical clearance of seven feet. Each required bicycle parking space must be accessible without moving another bicycle. There must be an aisle at least five feet wide between each set of bicycle parking to allow room for bicycle maneuvering.
2. The area devoted to bicycle parking must be surfaced as required for vehicle parking areas.
3. All long-term bicycle parking spaces must be located indoors or fully covered, such as by the use of an overhang or covered walkway, weatherproof outdoor bicycle lockers, or an indoor storage area. Where bicycle parking is not located within a building or locker, the cover design must be of permanent construction, designed to protect bicycles from rainfall, snow, and inclement weather, and with a minimum vertical clearance of seven feet.
4. Bicycle parking racks must permit the bicycle frame and one wheel to be locked to the rack and support the bicycle in a stable position. Structures that require a user-supplied locking device must be designed to accommodate U-shaped locking devices. All lockers and racks must be securely anchored to the ground or a structure to prevent the racks and lockers from being removed from the location.
5. If required bicycle parking facilities are not visible from the street or principal building entrance, signs must be posted indicating their location.

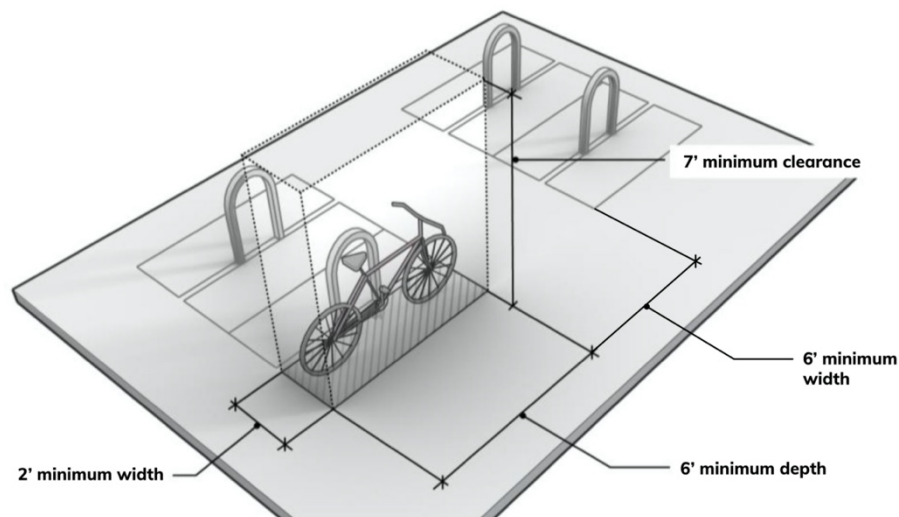
### B. Location

1. The bicycle parking area must be convenient to building entrances and street access, but may not interfere with normal pedestrian and vehicle traffic. Bicyclists must not be required to travel over stairs to access parking.
2. When required to provide bicycle spaces, certain uses are also required to provide long-term spaces, where bicycles will be left for longer periods of time and require a safe and weatherproof storage area. The required number of long-term spaces is shown as a percentage of the required total bicycle spaces in Table 10-C. All other required bicycle spaces must be designed as short-term spaces, which are areas where bicycles will be left for short stops, requiring a high degree of convenience. Nothing in this Ordinance prevents the provision of additional bicycle spaces in excess of that required; long-term spaces are required only in the amount calculated by the minimum number of spaces in Table 10-C, not of the total number of short-term spaces, which may exceed that required by the table.
3. Short-term bicycle parking spaces must be located no more than 50 feet from the principal building entrance and at the same grade as the sidewalk or an accessible route. Required bicycle parking spaces may be located in

the public right-of-way, with approval from the Department of Public Works, if one or more of the following conditions are met:

- a. The use does not provide vehicle parking on-site.
  - b. The addition of bicycle parking on the site would reduce the number of parking spaces below that required by this Ordinance.
  - c. Bicycle parking spaces in the right-of-way can be shared by uses located on the same blockface. In such cases, the number of bicycle spaces required is cumulative of that required by all uses sharing such spaces.
4. Required bicycle parking for residential uses may be provided in garages, storage rooms, and other resident-accessible, secure areas. Spaces within dwelling units or on balconies do not count toward satisfying bicycle parking requirements.

### BICYCLE PARKING DESIGN



## 10.8 REQUIRED OFF-STREET LOADING SPACES

### A. Design

1. All off-street loading spaces must be located on the same lot as the use served. With the exception of industrial districts, no off-street loading space must be located in a front or corner side yard or in front of a front building line.
2. All required off-street loading spaces must be at least 12 feet in width and at least 55 feet in length, exclusive of aisle and maneuvering space, and have a minimum vertical clearance of at least 14 feet.
3. All off-street loading spaces must be improved with a hard surfaced, all-weather dustless material.
4. All off-street loading spaces must meet the lighting requirements of Section 9.3.
5. When located adjacent to a residential districts, loading spaces must be screened along interior side and rear lot lines with a solid wall or fence, a minimum of six feet and a maximum of seven feet in height. Loading spaces should be located opposite any adjacent residential district lot line whenever possible.

**B. Minimum Number of Off-Street Loading Spaces**

Off-street loading spaces must be provided in accordance with the minimums of Table 10-E: Off-Street Loading Requirements. In the case of multi-tenant buildings or mixed-use developments, required loading spaces are calculated on the basis of each individual tenant (for example, if only one nonresidential use tenant of a multi-tenant building is over 10,000 square feet, only one loading space is required; if all tenants are under 10,000 square feet, no loading is required. Nothing herein prevents the construction of additional loading spaces above the minimums required here.

TABLE 10-E: OFF-STREET LOADING REQUIREMENTS	
Use Type	Minimum Number of Spaces Required
<b>Multi-Family Dwelling</b>	
Total of 50 dwelling units or more	1 loading space
<b>Commercial &amp; Institutional Use</b>	
10,000 - 100,000sf GFA	1 loading space
100,001 - 200,000sf GFA	2 loading spaces
200,001sf and above GFA	3 loading spaces
<b>Industrial Use</b>	
10,000 - 40,000sf GFA	1 loading spaces
40,001 - 100,000sf GFA	2 loading spaces
100,001 and above GFA	3 loading spaces

**10.9 COMMERCIAL AND RECREATIONAL VEHICLE STORAGE**

**A. Commercial Vehicles**

**1. Residential Lots**

a. No commercial vehicle may be parked outdoors on a residential lot, with the exception of vehicles engaged in loading or unloading or current work being done to the adjacent premises. This does not include standard size passenger motor vehicles (including, but not limited to, vans, sports utility vehicles (SUVs), standard passenger size livery vehicles, and pick-up trucks), which may be stored or parked outdoors overnight on lots in residential districts. Permitted vehicles also include those owned and used for commercial purposes by the occupant of a dwelling or guest, provided that the vehicle is stored or parked in a permitted parking area. Permitted commercial vehicles may include the logo of the commercial business painted on or applied to the vehicle.

b. All other commercial vehicles including, but not limited to, semi-truck tractor units, with or without attached trailers, commercial trailers, flatbed trucks, box vans and box trucks, buses, tow trucks, construction vehicles, livery vehicles that exceed standard passenger vehicle size, such as limousines, or other large commercial vehicles are not permitted to be stored or parked outdoors overnight on a residential lot.

**2. Nonresidential Lots**

On nonresidential lots, commercial vehicles with the logo of the commercial business painted on or applied to the vehicle that are being operated and stored in the normal course of business, such as signs located on delivery trucks, promotional vehicles, moving vans, and rental trucks, are permitted to be stored on the lot in areas related to their use as vehicles, provided that the primary purpose of such vehicles is not the display of signs. All such vehicles must be in operable condition.

**B. Recreational Vehicles**

1. Recreational vehicle or trailer licensed to transport recreational vehicles or equipment may be stored outdoors within the front or corner side yard if it is eight feet or less in height and 20 feet or less in length. Recreational vehicles or trailers that exceed these dimensions cannot be stored in the front or corner side yard.

2. Recreational vehicles must be stored within the interior side yard behind the front building line or in the rear yard. If a recreational vehicle and any trailer that is more than six feet in height as measured to the highest point of the vehicle, it must be located at least ten feet from any lot line.

- 3.** The area devoted to recreational vehicle storage must be on a hard, improved surface as required for vehicle parking areas.
- 4.** There is no limit on the storage of recreational vehicle within fully enclosed structures. Temporary storage tents do not meet the requirement of a fully enclosed structure.
- 5.** No recreational vehicle may be used for living, sleeping, housekeeping, or home occupation purposes in any district and may not be hooked up to any public utilities.
- 6.** All recreational vehicles must be maintained in mobile condition. No recreational vehicle may be parked or stored in such manner as to create a dangerous or unsafe condition on the lot where it is parked or stored. If the recreational vehicle is parked or stored, whether loaded or not, so that it may tip or roll, it is considered to be a dangerous and unsafe condition.